

RECLAMATION

Managing Water in the West

Categorical Exclusion Checklist

Contra Costa Canal Road Bridge Replacement Project

CEC-15-032

Prepared by:


Stacy L. Holt
Natural Resources Specialist
South-Central California Area Office

Date: 05 JANUARY 2016

Concurred by:

See Attachment A
Archaeologist/Architectural Historian
Mid-Pacific Regional Office
Regional Archeologist concurred with Item 8. Their determination has been placed within the project file.


Date: See Attachment A

Concurred by:


Shauna McDonald
Wildlife Biologist/Biology Technician
South-Central California Area Office

Date: 1/5/16

Concurred by:


Rain L. Emerson
Supervisory Natural Resources Specialist
South-Central California Area Office
ITA Designee concurred with Item 11. Their determination has been placed within the project file.

Date: 01/13/2016

Approved by:

for 
Michael Jackson
Area Manager
South-Central California Area Office

Date: 1/14/16



Background

Contra Costa County Public Works (County), in cooperation with the California Department of Transportation (Caltrans), proposes to replace the existing structurally deficient bridge (Bridge No. 28C0376) on Canal Road over the Contra Costa Canal (Canal); 0.5 miles west of Bailey Road, located in the north-central area of Contra Costa County, California near Bay Point (Figure 1). The legal description of the project location is: Lot 5 in Section 14, Township 2N, Range 1W, MDBM in Contra Costa County. The Contra Costa Canal milepost is approximately 20.26.

Built in 1938, the County-owned bridge is a single span, two-lane, reinforced concrete slab structure with the superstructure supported by reinforced concrete seat abutments on reinforced concrete spread footings. Caltrans has classified the bridge as structurally deficient in the event of a significant seismic event due to the condition of the substructure. The bridge also has a very low load capacity and rating due to its year of construction and remaining service life. The deck geometry does not meet standards for the traffic volumes present on the road, making this bridge functionally obsolete as well.

The Bureau of Reclamation (Reclamation) issued an easement in 1982 for the construction of a pedestrian bridge across the Contra Costa Canal at this location. With the proposed changes to the bridge (widening the lanes and shoulders and installing a pedestrian walkway on both sides) the pedestrian bridge will become redundant and is proposed for removal.

The canal and access roads are fenced and gated with standard chain link fence anchored with metal posts to preclude unauthorized access to the canal and canal access roads. Additional sections of chain link fence are attached to each side of the existing bridge that extend across both sides of the bridge and connect to the chain link fences of the canal and access roads. In order to remove the bridge and regrade the canal access roads, portions of the chain link fence and gates may need to be removed.

Funds for the Contra Costa Canal Road Bridge Replacement Project (Project) will be provided through the Federal Highway Bridge Program (HPB) and Local Agency Funding. The County is the lead agency for processing the Project pursuant to the California Environmental Quality Act and Caltrans is the assigned National Environmental Policy Act (NEPA) lead agency for the Project on behalf of the Federal Highway Administration.

As the Project is within Reclamation right-of-way (ROW), the County has requested permission to: (1) reconstruct existing approaches on Reclamation's canal service roads where they abut Canal Road; (2) reconstruct/raise canal service roads at all four corners of the bridge to conform between the raised road profile and the existing terrain, remove and replace in kind appurtenant canal service road fencing and gates; (3) temporarily use Reclamation ROW during bridge construction; and (4) provide a quit claim deed to vacate the pedestrian bridge easement upon removal of the pedestrian bridge.

Although Caltrans is completing NEPA for the entire Project as the Federal Highway Administration's designee, its NEPA documentation does not include Reclamation's federal

discretionary action. As such, Reclamation is preparing separate NEPA documentation to cover its federal discretionary actions.

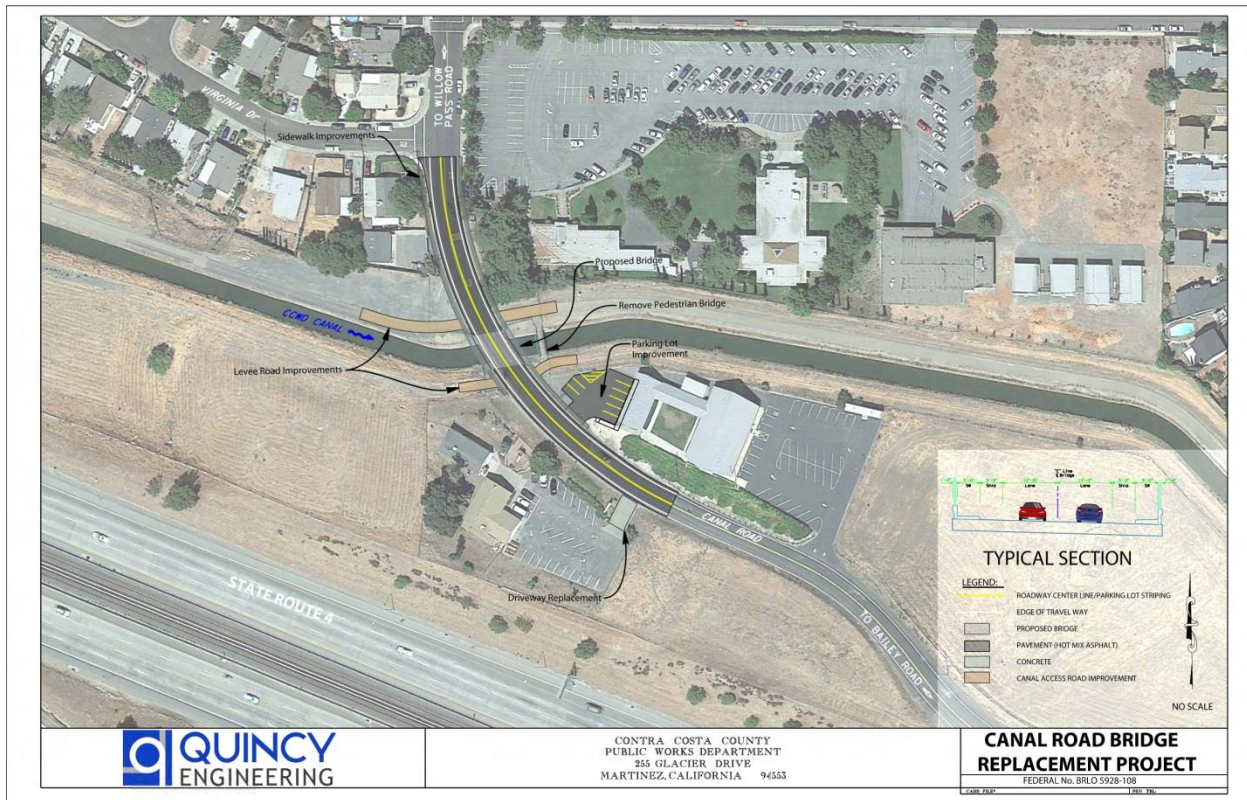


Figure 1 Canal Road Bridge

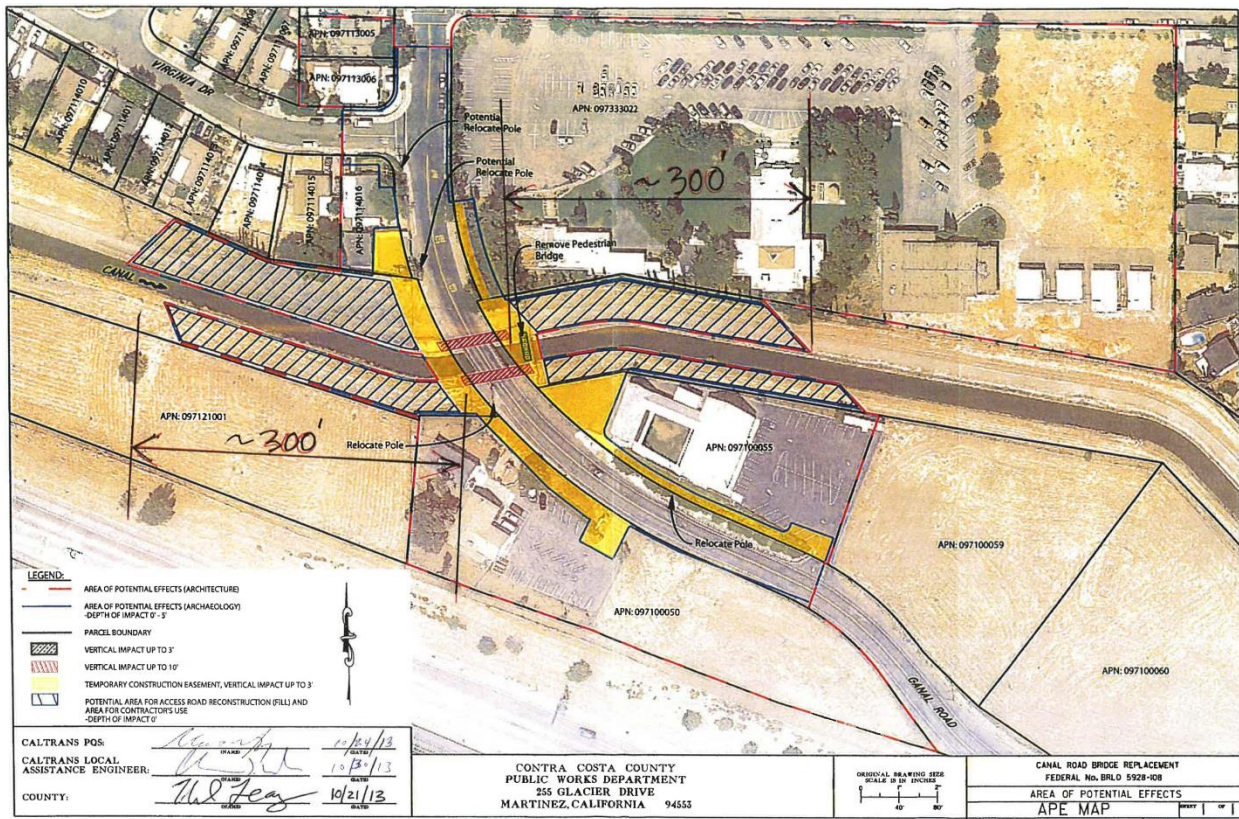
Nature of the Action

The Proposed Action includes two components: (1) acceptance of a quitclaim deed from the County to vacate the pedestrian bridge easement upon removal of the pedestrian bridge, and (2) issuance of land use authorization(s) by Contra Costa Water District to the County for construction of the Project, pursuant to their operating agreement (Contract No. 14-06-200-6072A) with Reclamation. Specific Project details are included below.

Caltrans, on behalf of the County, proposes to replace the Canal Road bridge (Bridge No. 28C0376) and remove the associated Pedestrian Bridge (Figure 2). Project activities would include:

- bridge replacement;
- raising the roadway and bridge profile up to 2.5 feet; reconstruction of the existing roadway approaches to accommodate the new height of the bridge;
- reconstruction/raising the canal maintenance access roads at all four corners of the bridge and adjacent driveways to conform between the raised road profile and the existing terrain;
- removal of Pedestrian Bridge and quit claim of easement;

- removal of and replacement in kind of the chain-link fence and gates from the bridge; and
- accompanying utility relocation and right-of-way acquisition.



The Project will have ground disturbing activities in the proposed construction limits on Reclamation's ROW (Figure 2). The majority of ground disturbance in this area would be from re-grading of the access roads. Depth of excavation in this area for fence removal and grading would be between three to five feet and is not expected to exceed five feet. Ground disturbance from removal of the chain link fence would result mainly from removal of the metal posts which are anchored into place with cement. Removal of existing canal chain link fencing shall be limited to the minimum necessary to perform the work. On completion of the project, the chain link fences and/or gates would be replaced in kind including across the new bridge, similar to the existing condition.

Temporary fencing will be installed around the perimeter of the construction area until the permanent fence is replaced to preclude unauthorized access to the canal and its access roads. In addition, high visibility orange construction fencing will delineate the project boundaries within the secured site to provide a visible barrier to keep construction staff from getting too close to the canal.

Pedestrian access across the existing pedestrian bridge will be maintained for as long as possible throughout construction. Contract specifications will require safe pedestrian access across the pedestrian bridge while preventing access to other construction areas.

Construction equipment would consist of asphalt pavers, sweepers, concrete mixers, forklifts, graders, loaders, pavement grinders, water trucks, rollers, cranes and various contractor trucks. It is anticipated that construction would begin in 2017 and take approximately 9 months to complete. During that time, the bridge and roadway would be closed at all times. Appropriate detour signs will clearly mark the route to the churches on the south side of the Canal.

Environmental Commitments

Caltrans shall implement the following environmental protection measures to avoid and/or reduce environmental consequences associated with the Proposed Action (Table 1).

Environmental consequences for resource areas assume the measures specified would be fully implemented.

Table 1. Environmental Commitments

Resource	Protection Measure
Biological Resources	Prior to any ground disturbance related to covered activities, a U.S. Fish & Wildlife Service/California Department of Fish and Wildlife Service (Service/CDFW)-approved biologist would conduct a preconstruction survey in areas identified in the planning surveys as having potential burrowing owl habitat. The surveys would establish the presence or absence of western burrowing owl and/or habitat features and evaluate use by owls in accordance with CDFW survey guidelines.
Biological Resources	On the parcel where the activity is proposed, the biologist would survey the proposed disturbance footprint and a 500-foot radius from the perimeter of the proposed footprint to identify burrows and owls. Adjacent parcels under different land ownership would not be surveyed.
Biological Resources	Surveys would take place near sunrise or sunset in accordance with CDFW guidelines.
Biological Resources	All burrows or burrowing owls would be identified and mapped.
Biological Resources	Surveys would take place no more than 30 days prior to construction.
Biological Resources	During the breeding season (February 1 – August 31), surveys would document whether burrowing owls are nesting in or directly adjacent to disturbance areas. During the nonbreeding season (September 1 – January 31), surveys would document whether burrowing owls are using habitat in or directly adjacent to any disturbance area. Survey results would be valid only for the season (breeding or nonbreeding) during which the survey is conducted.
Biological Resources	If burrowing owls are found during the breeding season (February 1 – August 31), all active nests that could be disturbed by the project would be avoided while the nest is occupied by young or adults. Avoidance would include establishment of a non-disturbance buffer zone. Construction may occur during the breeding season if a qualified biologist monitors the nest and determines that the birds have not begun egg-laying and incubation or that the juveniles from the occupied burrows have fledged. During the nonbreeding season (September 1 – January 31), the project proponent would avoid the owls and the burrows they are using, if possible. Avoidance would include the establishment of a buffer zone.
Biological Resources	If occupied burrows cannot be avoided, passive relocation would be implemented if it is outside the breeding season (September 1 – January 31), if a qualified biologist has determined that the birds have not begun egg-laying and incubation, or once the young have fledged. Owls would be passively relocated from burrows in the immediate impact zone and within a 160-foot buffer zone by installing one-way doors in burrow entrances. These doors would be in place for 48 hours prior to excavation. The project area would be monitored daily for one week to confirm that the owl has abandoned the burrow. Whenever possible, burrows would be excavated using hand tools and refilled to prevent reoccupation. Plastic tubing or a similar structure would be inserted in the tunnels during excavation to maintain an escape route for any owls inside the burrow. If no burrowing owls are observed, burrows would be filled in or the area would be disked to prevent owls from occupying the area.
Biological Resources	If tree removal, pruning, or grubbing activities are necessary, such activities should be conducted in the fall or winter after August 31 and before February 1. This timing would avoid impacts to nesting birds during the breeding season (February 1 to August 31).

Resource	Protection Measure
Biological Resources	If project construction, including tree removal, begins during the breeding season (February 1 to August 31), preconstruction surveys should be conducted within the project footprint and a 250 foot buffer, by a qualified biologist no more than two weeks prior to equipment or material staging, pruning/grubbing or surface-disturbing activities. If no active nests are found, no further avoidance is necessary. If work ceases for a period of two weeks or longer, preconstruction nesting bird surveys should be conducted prior to recommencing work.
Biological Resources	If active nests (i.e., nests with eggs or young birds present) are found, non-disturbance buffers should be established at a distance sufficient to minimize disturbance based on the nest location, topography, cover, the nesting pair's tolerance to disturbance and the type/duration of potential disturbance. No work should occur within the non-disturbance buffers until the young have fledged. Buffer size should be determined in cooperation with the CDFW and the Service. If buffers are established and it is determined that project activities are resulting in nest disturbance, work should cease immediately and the CDFW and the Service should be contacted for further guidance.
Biological Resources	If necessary, project proponents would undertake construction monitoring during project implementation to ensure that measures required to avoid and minimize impacts on covered species and natural communities are properly implemented. Resources identified in planning or preconstruction surveys would be the focus of construction monitoring efforts. Construction monitoring would be conducted by qualified biologists. Before implementing a covered activity, the project proponent would develop and submit a construction monitoring plan to the Habitat Conservation Plan (HCP) for approval.
Biological Resources	All staging areas and borrow sites would be returned to preconstruction conditions.
Biological Resources	Erosion Control Measures for this project shall be designed to prevent the spread of invasive plant species. Landscaping designs for this project shall not contain invasive species in the plant selections or seed mixtures. Construction equipment shall be cleaned before mobilizing to arrive at the project site and before leaving the project site.
Biological Resources	<p>Erosion Control Measures shall be implemented during construction. To minimize the mobilization of sediment to adjacent water bodies, the following erosion-control and sediment-control measures would be included in the Storm Water Pollution Prevention Plan (SWPPP) to be included in the construction specifications, based on standard County measures and standard dust-reduction measures.</p> <ul style="list-style-type: none"> • Soil exposure would be minimized through the use of temporary best management practices (BMPs), groundcover, and stabilization measures; • All stockpile areas within 200 feet of the canal would be surrounded by a filter fabric fence and interceptor dike. Side slopes shall not be steeper than 2:1; • Where appropriate, bare areas would be covered with mulch and cleared areas would be revegetated with native species; and, • The contractor shall conduct periodic maintenance of erosion- and sediment-control measures.
Biological Resources	<p>To conform to water quality requirements, a SWPPP would be required, and would include the following:</p> <ul style="list-style-type: none"> • Vehicle maintenance and staging and storing equipment, materials, fuels, lubricants, solvents, and other possible contaminants shall be outside of the Canal. Any necessary equipment washing shall occur where the water cannot flow into the Canal; • Construction equipment would not be operated in flowing water; • Construction work shall be conducted according to site-specific construction plans that minimize the potential for sediment input to the Canal; • Raw cement, concrete or concrete washings, asphalt, paint or other coating material, oil or other petroleum products, or any other substances that could be hazardous to aquatic life shall be prevented from contaminating the soil or entering the Canal; • Equipment used in and around the Canal shall be in good working order and free of dripping or leaking engine fluids; and, • Any surplus concrete rubble, asphalt, or other rubble from construction shall be taken to an approved disposal site.

Resource	Protection Measure
Biological Resources	Methods would be used during construction activities to ensure that bridge railing materials would not enter waters of the United States or State; no material shall be allowed to enter the waters of the United States or State during demolition or construction. These methods may include netting, scaffolding, or some other barrier to catch falling debris.
Biological Resources	Where appropriate, the project boundaries including the temporary and permanent construction impact areas, shall be staked with an Environmentally Sensitive Area (ESA) fencing in order to contain construction activities. The staking and ESA fencing shall be done in coordination with a biologist, who is qualified and knowledgeable of biological resources in the action area.
Biological Resources	All trash shall be kept in wildlife-proof receptacles, and no non-natural food or water would be left unattended for the duration of the project construction. All trash items such as wrappers, cans, bottles, and food scraps shall be disposed of in closed containers and removed at least once a week from the project site.
Biological Resources	No firearms shall be allowed on the project site.
Biological Resources	To prevent harassment or mortality of wildlife by dogs or cats, no pets would be permitted on the project site.
Biological Resources	An employee education program shall be conducted for construction personnel. The program shall consist of a brief presentation by persons knowledgeable in burrowing owl and legislative protection to explain species concerns to contractors, their employees, and agency personnel involved in the project. The program shall include the following: a description of burrowing owls and their habitat needs; a report of the occurrence of burrowing near the project area; an explanation of the status of the species and its protection; and a list of measures being taken to reduce impacts to the species during project construction and implementation. A fact sheet conveying this information shall be prepared for distribution to the above-mentioned people and anyone else who may enter the project site.
Water Resources	All necessary permits will be acquired prior to beginning construction in waterways.
Water Resources	Conditions of all permits will be observed as stipulated.

Exclusion Category

516 DM 14.5 paragraph D (10): *Issuance of permits, licenses, easements, and crossing agreements which provide right-of-way over Bureau lands where the action does not allow for or lead to a major public or private action.*

Evaluation of Criteria for Categorical Exclusion

- | | | | |
|--|---|---------------------------------------|---------------------------------|
| 1. This action would have a significant effect on the quality of the human environment (40 CFR 1502.3). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |
| 2. This action would have highly controversial environmental effects or involve unresolved conflicts concerning alternative uses of available resources (NEPA Section 102(2)(E) and 43 CFR 46.215(c)). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |
| 3. This action would have significant impacts on public health or safety (43 CFR 46.215(a)). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |

- | | No | Uncertain | Yes |
|---|---|---------------------------------------|---------------------------------|
| 4. This action would have significant impacts on such natural resources and unique geographical characteristics as historic or cultural resources; parks, recreation, and refuge lands; wilderness areas; wild or scenic rivers; national natural landmarks; sole or principal drinking water aquifers; prime farmlands; wetlands (EO 11990); flood plains (EO 11988); national monuments; migratory birds; and other ecologically significant or critical areas (43 CFR 46.215 (b)). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |
| 5. This action would have highly uncertain and potentially significant environmental effects or involve unique or unknown environmental risks (43 CFR 46.215(d)). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |
| 6. This action would establish a precedent for future action or represent a decision in principle about future actions with potentially significant environmental effects (43 CFR 46.215 (e)). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |
| 7. This action would have a direct relationship to other actions with individually insignificant but cumulatively significant environmental effects (43 CFR 46.215 (f)). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |
| 8. This action would have significant impacts on properties listed, or eligible for listing, on the National Register of Historic Places as determined by Reclamation (LND 02-01) (43 CFR 46.215 (g)). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |
| 9. This action would have significant impacts on species listed, or proposed to be listed, on the List of Endangered or Threatened Species, or have significant impacts on designated critical habitat for these species (43 CFR 46.215 (h)). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |
| 10. This action would violate a Federal, tribal, State, or local law or requirement imposed for protection of the environment (43 CFR 46.215 (i)). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |
| 11. This action would affect ITAs (512 DM 2, Policy Memorandum dated December 15, 1993). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |
| 12. This action would have a disproportionately high and adverse effect on low income or minority populations (EO 12898) (43 CFR 46.215 (j)). | No
<input checked="" type="checkbox"/> | Uncertain
<input type="checkbox"/> | Yes
<input type="checkbox"/> |

- | | No | Uncertain | Yes |
|--|-------------------------------------|--------------------------|--------------------------|
| 13. This action would limit access to, and ceremonial use of, Indian sacred sites on Federal lands by Indian religious practitioners or significantly adversely affect the physical integrity of such sacred sites (EO 13007, 43 CFR 46.215 (k), and 512 DM 3)). | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| 14. This action would contribute to the introduction, continued existence, or spread of noxious weeds or non-native invasive species known to occur in the area or actions that may promote the introduction, growth, or expansion of the range of such species (Federal Noxious Weed Control Act, EO 13112, and 43 CFR 46.215 (l)). | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |

NEPA Action: Categorical Exclusion

The Proposed Action is covered by the exclusion category and no extraordinary circumstances exist. The Action is excluded from further documentation in an EA or EIS.

Attachment A: Cultural Resources Determination

CULTURAL RESOURCES COMPLIANCE

Division of Environmental Affairs

Cultural Resources Branch (MP-153)

MP-153 Tracking Number: 15-SCAO-041

Project Name: National Historic Preservation Act Compliance for Caltrans Canal Road Bridge Replacement Project over the Contra Costa Canal, Contra Costa County, California

NEPA Document: CEC 15-032

NEPA Contact: Stacy Holt, Natural Resource Specialist

MP 153 Cultural Resources Reviewer: BranDee Bruce, Architectural Historian

Date: December 21, 2015

The California Department of Transportation (Caltrans) proposes to replace the Canal Road Bridge across the Contra Costa Canal (CCC) in Contra Costa County, California. The CCC was constructed and is owned by the Bureau of Reclamation. Caltrans will require a land use authorization from Reclamation to work within Reclamation's right-of-way. In addition, a quit claim deed will need to be accepted by Reclamation for a pedestrian bridge which will be removed as part of the undertaking. The proposed action constitutes an undertaking with the potential to cause effects to historic properties, assuming such properties are present, requiring compliance with Section 106 of the National Historic Preservation Act (NHPA) as amended. Caltrans was designated lead federal agency to act on behalf of Reclamation for purposes of compliance with Section 106 of the NHPA for this undertaking (see attached email).

Based on historic properties identification efforts conducted by Caltrans, the CCC was identified as the only eligible historic property within the area of potential effect (APE), with the Canal Road Bridge identified as a contributing component to the CCC. Caltrans applied the criteria of adverse effect to the CCC and proposed a finding of No Adverse Effect with Non Standard Conditions pursuant to 36 CFR §800.5(b). Caltrans consulted with the State Historic Preservation Officer (SHPO) by sending consultation packages on January 16, 2015 and September 22, 2015. SHPO concurred with all their findings in letters dated February 19, 2015, and November 3, 2015 (see attached concurrence letters). Reclamation accepts the findings and outcome of the consultation for the proposed activities on Reclamation land that involve Reclamation facilities.

Consultation correspondence between Caltrans and the SHPO, and Reclamation's letter designating Caltrans as lead federal agency has been provided with this cultural resources compliance document for inclusion in the administrative record for this action. This document serves as notification that Section 106 compliance has been completed for this undertaking. Please note that if project activities subsequently change, additional NHPA Section 106 review, including further consultation with the SHPO, may be required.

Attachments:

Letter: Caltrans to SHPO dated January 16, 2015

Letter: SHPO to Caltrans dated February 19, 2015

Letter: Reclamation to SHPO, designating Caltrans lead, dated July 21, 2015

Letter: Caltrans to SHPO dated September 22, 2015

Letter: SHPO to Caltrans dated November 3, 2015

DEPARTMENT OF TRANSPORTATION

111 GRAND AVENUE
P.O. BOX 23660
OAKLAND, CA 94623-0660
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TTY 711
www.dot.ca.gov



*Serious drought
Help save water!*

January 16, 2015

Ms. Carol Roland-Nawi, Ph.D
State Historic Preservation Officer
Office of Historic Preservation
1725 23rd Street, Suite 100
Sacramento, CA 95816

Dear Ms. Roland-Nawi:

The California Department of Transportation (Caltrans) is initiating consultation with the State Historic Preservation Officer (SHPO) regarding the Canal Road Bridge replacement project (Undertaking) in the community of Bay Point in Contra Costa County. On behalf of the Federal Highway Administration (FHWA), Caltrans is proposing to replace the Canal Road Bridge (No. 28C0376) over the Contra Costa Canal. Associated work includes raising and reconstructing roadway approaches at each end of the bridge. A full project description can be found on page 1 of the enclosed Historic Property Survey Report (HPSR).

Section 106 responsibilities for this Undertaking have been conducted in accordance with the January 2014 *First Amended Programmatic Agreement Among the Federal Highway Administration, the Advisory Council on Historic Preservation, the California State Historic Preservation Officer, and the California Department of Transportation Regarding Compliance with Section 106 of the National Historic Preservation Act, as it Pertains to the Administration of the Federal-Aid Highway Program in California* (hereafter, the PA).

Enclosed you will find an HPSR with an attached resource record and an Archaeological Survey Report (ASR) for the proposed Undertaking. The HPSR summarizes: 1) our determination of the Area of Potential Effects (APE); 2) an identification of historic properties located within the Undertaking's APE; and, 3) an evaluation of historic properties for inclusion on the National Register of Historic Places (NRHP).

Identification efforts for this Undertaking identified two resources within the APE—the Contra Costa Canal and the Canal Road Bridge.

The first resource, the Contra Costa Canal (P-07-2695), was determined eligible for inclusion in the National Register of Historic Places (NRHP) through a separate undertaking on March 9, 2005.

The second resource, the Canal Road Bridge (No. 28C0376, P-07-2648), was previously evaluated as a contributor to the Contra Costa Canal but this eligibility determination did not receive SHPO concurrence. The Canal Road Bridge is recorded and evaluated in an attachment to the HPSR.

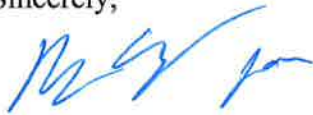
Ms. Carol Roland-Nawi, Ph.D
January 16, 2015

We are consulting with you at this time under Stipulation VIII.C.6 of the PA to obtain concurrence on the eligibility determination for the Canal Road Bridge. The Canal Road Bridge has been determined *eligible* for inclusion in the NRHP as a contributor to the Contra Costa Canal as a result of this study.

An ASR was prepared for this Undertaking with negative results and is included in the attached documentation.

We would appreciate receiving the SHPO's response regarding the determination of eligibility within 30 days of your receipt of this submittal. If you have any questions, please contact me at (510) 622-5458 or lissa.mckee@dot.ca.gov. Thank you for your assistance with this Undertaking.

Sincerely,



ELIZABETH MCKEE
Chief, Office of Cultural Resources Studies
Caltrans District 4

Attachments:

Historic Property Survey Report for the Canal Road Bridge Replacement Project, Bay Point, Contra Costa County, California

ASR Short Form for Local Assistance Projects for the Canal Road Bridge Replacement Project, Bay Point, Contra Costa County, California

cc: Anmarie Medin, Chief, Caltrans Headquarters Cultural Studies Office (CSO)
Kelly Hobbs, Branch Chief, Section 106 Coordination, CSO
OLA files
HRC files

**OFFICE OF HISTORIC PRESERVATION
DEPARTMENT OF PARKS AND RECREATION**

1725 23rd Street, Suite 100
SACRAMENTO, CA 95816-7100
(916) 445-7000 Fax: (916) 445-7053
calshpo@parks.ca.gov
www.ohp.parks.ca.gov



February 19, 2015

Reply To: FHWA_2015_0120_001

Elizabeth McKee
Chief, Office of Cultural Resource Studies
Caltrans District 4
PO Box 23660
Oakland, CA 94623-0660

Re: Determinations of Eligibility for the Proposed Canal Road Bridge Replacement Project, Bay Point, Contra Costa County, CA

Dear Ms. McKee:

Thank you for consulting with me about the subject undertaking in accordance with the January 1, 2014 *First Amended Programmatic Agreement Among the Federal Highway Administration, the Advisory Council on Historic Preservation, the California State Historic Preservation Officer, and the California Department of Transportation Regarding Compliance with Section 106 of the National Historic Preservation Act, as it Pertains to the Administration of the Federal-Aid Highway Program in California* (PA).

Caltrans has determined that Canal Road Bridge (No. 28C0376, P-07-2648) is contributor to the Contra Costa Canal, a property previously determined eligible for the National Register of Historic Places (NRHP) in 2005. The bridge is considered a contributor because: 1) it was constructed during the period of significance of the Contra Costa Canal; 2) it was built during the same era as the construction of the Canal and with the express purpose of carrying traffic over the canal; and 3) because the bridge retains sufficient integrity to convey the significance of the Contra Costa Canal. The bridge is not individually eligible for the NRHP.

Based on my review of the submitted documentation I concur with the foregoing determinations.

Thank you for considering historic properties during project planning. If you have any questions, please contact Natalie Lindquist of my staff at (916) 445-7014 or email at natalie.lindquist@parks.ca.gov.

Sincerely,

A handwritten signature in black ink, reading "Carol Roland-Nawi, Ph.D.".

Carol Roland-Nawi, Ph.D.
State Historic Preservation Officer



United States Department of the Interior

BUREAU OF RECLAMATION
Mid-Pacific Regional Office
2800 Cottage Way
Sacramento, CA 95825-1898

IN REPLY REFER TO:

MP-153
ENV-3.00

JUL 21 2015

CERTIFIED - RETURN RECEIPT REQUESTED

Ms. Julianne Polanco
State Historic Preservation Officer
Office of Historic Preservation
1725 23rd Street, Suite 100
Sacramento, CA 95816

Subject: National Historic Preservation Act (NHPA) Section 106 Compliance for the Canal Road Bridge Replacement Project, Bay Point, Contra Costa County, California
(15-SCAO-041/FHWA_2015_0120-001)

Dear Ms. Polanco:

Contra Costa County (County) proposes to replace the structurally deficient Canal Road Bridge that crosses the Contra Costa Canal (CCC). As owner of the CCC, the Bureau of Reclamation must approve modifications that may result from the bridge replacement. The County also applied for funding from the Federal Highway Administration (FHWA) through the California Department of Transportation (Caltrans). Reclamation's approval is subject to compliance with Title 54 USC § 306108, formerly and commonly known as Section 106 of the NHPA, and its implementing regulations found at 36 CFR Part 800. Caltrans has initiated Section 106 consultations with your office regarding this project with the submission of a determination of eligibility for the Canal Road Bridge, receiving concurrence dated February 19, 2015. Reclamation recently received an application from the County for this project, after cultural resources work had been started through their coordination with Caltrans. Since both Federal agencies have undertakings for this project, Reclamation designates FHWA (with Caltrans acting as their agent) as lead Federal agency to act on our behalf to fulfill collective responsibilities under Section 106, pursuant to 36 CFR § 800.2(a)(2).

Reclamation has been working closely with Caltrans, the County, and the County's consultant on a finding of effect for this project. As lead agency, Caltrans will make the submission when ready. If you have any questions regarding this project, please contact Ms. Brandee Bruce, Historian, at bbruce@usbr.gov or 916-978-5039.

Sincerely,

Anastasia T. Leigh
Regional Environmental Officer

cc: See Next Page.

cc: Ms. Elizabeth McKee
Chief, Office of Cultural Resource Studies
Caltrans District 4
P.O. Box 23660
Oakland, CA 94623-0660

DEPARTMENT OF TRANSPORTATION

DIVISION OF ENVIRONMENTAL ANALYSIS

1120 N STREET P.O. BOX 942873, MS-27

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SEP 22 2015

OHP

Serious drought.
Help save water!

September 22, 2015

04-CC-LSR

EA: BRLO 5928-108

FHWA_2015_0120_001

Julianne Polanco
State Historic Preservation Officer
Office of Historic Preservation
1725 23rd Street, Suite, 100
Sacramento, CA 95816

Subject: Re: Section 106 Consultation for the Finding of No Adverse Effect for the Replacement of Canal Road Bridge in Contra Costa County.

Dear Ms. Polanco,

The California Department of Transportation, (Caltrans) is seeking concurrence from the State Historic Preservation Officer (SHPO) for a finding of No Adverse Effect regarding the replacement of Canal Road Bridge in Contra Costa County.

Caltrans is continuing consultation as part of its NEPA assignment of federal responsibilities by the Federal Highway Administration (FHWA), effective October 1, 2012 and pursuant to 23 USC 326 and 327.

Enclosed you will find one copy of the Finding of Effect (FOE) for the proposed project documenting cultural resources compliance with Section 106 in accordance with the January 2014 *First Amended Programmatic Agreement Among the Federal Highway Administration, the Advisory Council on Historic Preservation, the California State Historic Preservation Officer, and the California Department of Transportation Regarding Compliance with Section 106 of the National Historic Preservation Act, as it Pertains to the Administration of the Federal-Aid Highway Program in California* (hereafter, the PA), CEQA, and PRC §5024.5. The Area of Potential Effects (APE) is depicted Attachment 1 of the FOE.

With this project, Contra Costa County Public Works, with coordination with the California Department of Transportation (Caltrans) and the United States Bureau of Reclamation (Reclamation), and with assistance from the Federal Highway Administration (FHWA), proposes to replace Canal Road Bridge (28C0376), which crosses the Contra Costa Canal.

One historic district was identified within the APE. The Contra Costa Canal Historic District (canal) received concurrence in September 2002, and in 2005 nine Reclamation bridges were evaluated with three being found contributing, one of which the Somerville Road Bridge

received concurrence in February 2005. Canal Road Bridge received concurrence as a contributing feature to the canal for this project in April 2014.

As documented in the FOE, Caltrans has made a finding of No Adverse Effect (without Standard Conditions) under the PA. Caltrans is requesting that the SHPO concur within 30 days of receipt of this transmittal.

It is our intent to make a de minimis impact determination, under Section 4(f) of the Department of Transportation Act of 1966, based on your concurrence with the finding of No Adverse Effect for the undertaking. Caltrans will consider a non-response as written concurrence with the de minimis determination.

If you need any additional information, please do not hesitate to contact Helen Blackmore, District 4, Associate Environmental Planner – Architectural History, at (510) 286-6744 or Helen.Blackmore@dot.ca.gov or Noah M. Stewart, Branch Chief SFOBB, Local Assistance & Maintenance Architectural History, at (510) 286-5370 or Noah.Stewart@dot.ca.gov.

Thank you for your assistance with this project.

Sincerely,



Kelly Hobbs
Chief, Section 106 Coordination Branch
Caltrans, Division of Environmental Analysis

Attachments: Finding of No Adverse Effect for the Canal Road Bridge Replacement Project, Contra Costa, CA BRLO 5928-108.

cc: Noah Stewart, OCRS
HRC files

**OFFICE OF HISTORIC PRESERVATION
DEPARTMENT OF PARKS AND RECREATION**

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November 3, 2015

Reply To: FHWA_2015_0120_001

Jody Brown, Chief
Cultural Studies Office
Caltrans Division of Environmental Analysis
PO Box 942874
Sacramento, CA 94274-0001

Re: Finding of Effect for the Proposed Replacement of the Canal Road Bridge in
Contra Costa County, CA

Dear Ms. Brown:

You are consulting with me about the subject undertaking in accordance with the January 2014 *First Amended Programmatic Agreement Among the Federal Highway Administration, the Advisory Council on Historic Preservation, the California State Historic Preservation Officer, and the California Department of Transportation Regarding Compliance with Section 106 of the National Historic Preservation Act, as it Pertains to the Administration of the Federal-Aid Highway Program in California* (PA).

Contra Costa County Public Works, with coordination with Caltrans and the United States Bureau of Reclamation (BUR), and with assistance from the Federal Highway Administration, proposes to replace the Canal Road Bridge (28C0376), which crosses the Contra Costa Canal, and accept the quitclaim deed for the county owned pedestrian bridge removal and land use authorization. The BUR has designated Caltrans as the lead agency.

One historic district is located within the area of potential effect (APE) for the undertaking. The Contra Canal Historic District was determined eligible in September 2002, and in 2005 nine BUR bridges were evaluated with three being found contributing, one of which was the Somerville Road Bridge. The Canal Road Bridge was found to be a contributing feature to the canal in April 2014.

Caltrans has found that the undertaking as proposed will have no adverse on the Contra Costa Canal Historic District. The canal is significant under Criterion A as a water conveyance system integral to the development and economy at a local level, and it will continue to be distinguishable as such with the replacement of the Canal Road Bridge. The removal of the bridge will have an effect on a small part of the Canal's materials, as well as the design because it will remove a contributing element. However the overall effect will not be adverse, as defined in 36 CFR 800.5(a)1, because after the replacement, the Canal will still retain the integrity of association, location, design, materials, and workmanship. The bridge replacement will not affect the Canal's historic association with the Central Valley Project or economic growth of eastern Contra Costa County. In addition the project will not physically impact the channel lining of the Contra Costa Canal.

Ms. Brown
November 3, 2015
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Based on my review of the submitted documentation, I concur that the project as proposed will have no adverse effect on historic properties.

Thank you for considering historic properties during project planning. If you have any questions, please contact Natalie Lindquist of my staff at (916) 445-7014 or email at natalie.lindquist@parks.ca.gov.

Sincerely,

A handwritten signature in blue ink, appearing to be 'J. Polanco', with a long horizontal line extending to the right.

Julianne Polanco
State Historic Preservation Officer