

# RECLAMATION


## *Managing Water in the West*

### Categorical Exclusion Checklist

## Caltrans State Route 201 Bridge Widening and Modification Project

**CEC-14-052**

Prepared by:

  
Stacy L. Holt  
Natural Resources Specialist  
South-Central California Area Office

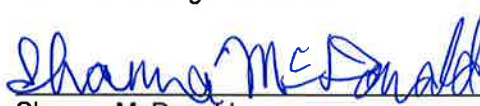
Date: 08/11/2011

Concurred by:

See Attachment A  
Archaeologist/Architectural Historian  
Mid-Pacific Regional Office


Date: See Attachment A

Concurred by:

  
Shauna McDonald  
Wildlife Biologist/Biology Technician  
South-Central California Area Office

Date: 8/13/15

Concurred by:

  
Rain L. Emerson  
Supervisory Natural Resources Specialist  
South-Central California Area Office

Date: 08/17/2015

Approved by:

 Soe  
Michael Jackson  
Area Manager  
South-Central California Area Office

Date: 8/26/2015





## Background

The California Department of Transportation (Caltrans) has proposed a project to upgrade three bridges on State Road 201 in Tulare County, California. The bridges are located on State Road 201 at the crossings of Sand Creek, the Friant-Kern Canal, and State Road 216. Proposed improvements consist of widening the bridge decks and upgrading railings in accordance with modern safety standards.

Caltrans has requested a temporary land use authorization from the Bureau of Reclamation (Reclamation) for work on the State Road 201 bridge, as it is located over the Friant-Kern Canal. The crossings at Sand Creek and State Road 216 do not involve Reclamation facilities and do not require Reclamation approval.

## Nature of the Action

Reclamation will issue a land use authorization to Caltrans for an area roughly 50 feet wide by 200 feet long on the south side of the bridge located on State Road 201 over the Friant Kern-Canal, as shown in Attachment B.

The location of the proposed work is shown in Figure 1. Planned improvements include:

- removing existing bridge railings and replacing them with standard concrete rails;
- widening the bridge structure by 6 feet 5 inches on either side to become 44 feet wide in total;
- adding four new ~24 square inch bridge piers, one on each side of the existing two bents;
- widening the bridge approach on both ends, tapering from the wider bridge to the existing roadway;
- realigning and regrading access roads along both sides of the Friant-Kern Canal slightly;
- installing metal beam guardrails at the bridge approaches;
- removing and replacing existing pavement on the bridge; and
- upgrading the drainage system to direct stormwater to the southwest, away from the canal.

A 10-inch Stone Corral Irrigation District water line is currently attached to the north side of the bridge. The water line will be relocated to the south side of the bridge while crews are working on the north side. It will then be returned to the north side in the second stage of construction, while the south side is being widened. There are also AT&T utility marker posts located north of the bridge, and a cable line associated with the marker posts. The AT&T facilities will be relocated to the north, a minimum of 8 feet from their current location, or 20 feet from the travel way.

During construction, equipment would be staged in an existing pullout just northwest of the bridge.

## Environmental Commitments

Caltrans shall implement the following environmental protection measures to avoid and/or reduce environmental consequences associated with the Proposed Action (Table 1).

Environmental consequences for resource areas assume the measures specified would be fully implemented.

Table 1 Environmental Commitments

Resource	Protection Measure
Biological	Caltrans would follow the avoidance measures from the U.S. Fish and Wildlife Service (Service 2011) San Joaquin kit fox measures for the work at the Friant-Kern Canal crossing.
Biological	Caltrans would install exclusionary material along the Friant-Kern Canal bridge prior to February 15 of the year that work starts to prevent nesting activity of Cliff Swallows to avoid any take (as defined under the Migratory Bird Treaty Act). The material would be repaired or replaced if necessary.
Cultural	Caltrans shall comply with all measures required by the State Historic Preservation Officer pursuant to their programmatic agreement. (See Attachment A)

## Exclusion Category

516 DM 14.5 paragraph D (10): *Issuance of permits, licenses, easements, and crossing agreements which provide right-of-way over Bureau lands where the action does not allow for or lead to a major public or private action.*

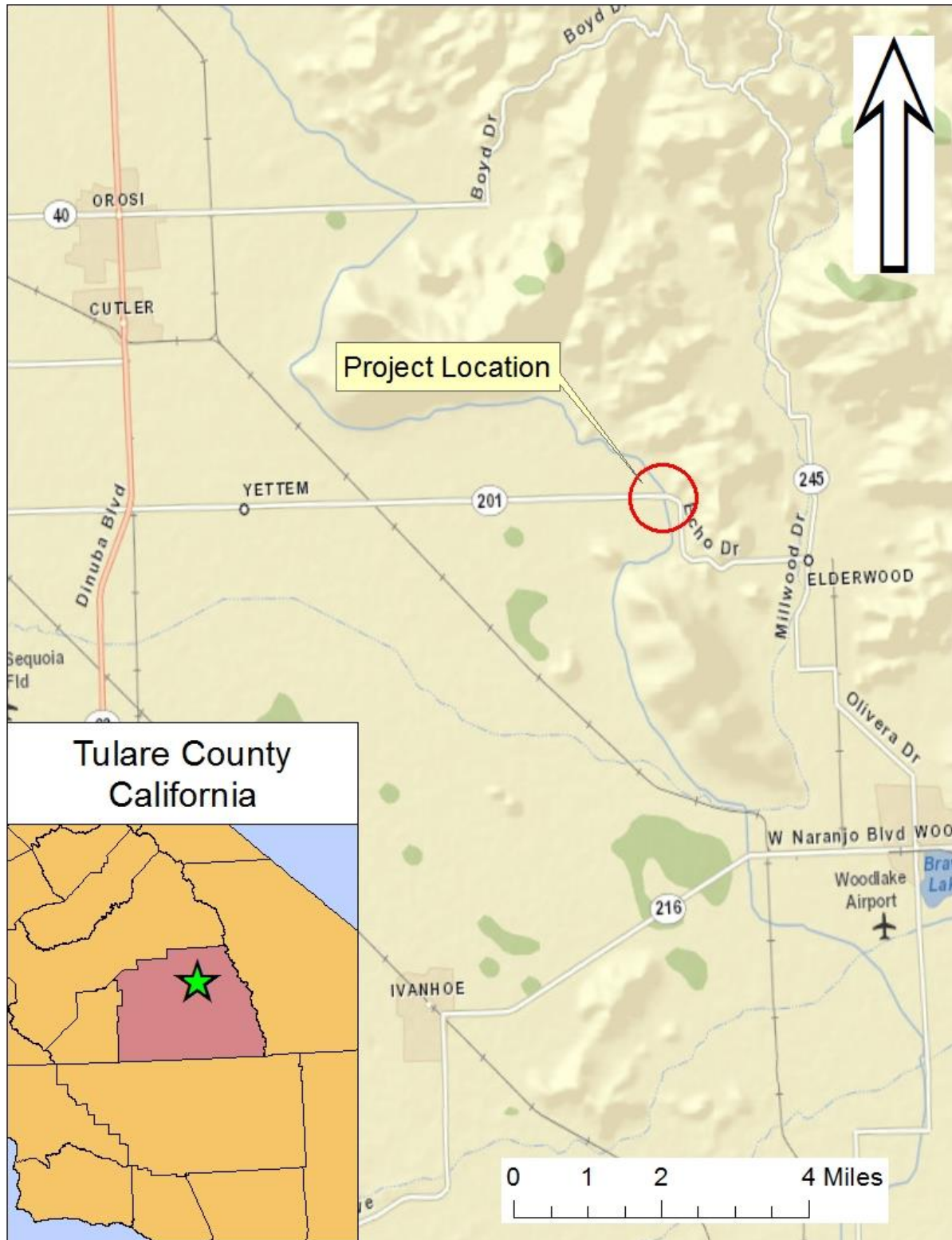


Figure 1 - Project Location

## Evaluation of Criteria for Categorical Exclusion

- |   |   |                                       |                                 |
|---|---|---------------------------------------|---------------------------------|
| 1. This action would have a significant effect on the quality of the human environment (40 CFR 1502.3).   | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 2. This action would have highly controversial environmental effects or involve unresolved conflicts concerning alternative uses of available resources (NEPA Section 102(2)(E) and 43 CFR 46.215(c)).  | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 3. This action would have significant impacts on public health or safety (43 CFR 46.215(a)).  | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 4. This action would have significant impacts on such natural resources and unique geographical characteristics as historic or cultural resources; parks, recreation, and refuge lands; wilderness areas; wild or scenic rivers; national natural landmarks; sole or principal drinking water aquifers; prime farmlands; wetlands (EO 11990); flood plains (EO 11988); national monuments; migratory birds; and other ecologically significant or critical areas (43 CFR 46.215 (b)). | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 5. This action would have highly uncertain and potentially significant environmental effects or involve unique or unknown environmental risks (43 CFR 46.215(d)).   | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 6. This action would establish a precedent for future action or represent a decision in principle about future actions with potentially significant environmental effects (43 CFR 46.215 (e)).  | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 7. This action would have a direct relationship to other actions with individually insignificant but cumulatively significant environmental effects (43 CFR 46.215 (f)).  | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 8. This action would have significant impacts on properties listed, or eligible for listing, on the National Register of Historic Places as determined by Reclamation (LND 02-01) (43 CFR 46.215 (g)).  | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 9. This action would have significant impacts on species listed, or proposed to be listed, on the List of Endangered or Threatened Species, or have significant impacts on designated critical habitat for these species (43 CFR 46.215 (h)).   | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |

- |  |   |                                       |                                 |
|--|---|---------------------------------------|---------------------------------|
| 10. This action would violate a Federal, tribal, State, or local law or requirement imposed for protection of the environment (43 CFR 46.215 (i)).   | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 11. This action would affect ITAs (512 DM 2, Policy Memorandum dated December 15, 1993).   | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 12. This action would have a disproportionately high and adverse effect on low income or minority populations (EO 12898) (43 CFR 46.215 (j)).  | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 13. This action would limit access to, and ceremonial use of, Indian sacred sites on Federal lands by Indian religious practitioners or significantly adversely affect the physical integrity of such sacred sites (EO 13007, 43 CFR 46.215 (k), and 512 DM 3)).   | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |
| 14. This action would contribute to the introduction, continued existence, or spread of noxious weeds or non-native invasive species known to occur in the area or actions that may promote the introduction, growth, or expansion of the range of such species (Federal Noxious Weed Control Act, EO 13112, and 43 CFR 46.215 (l)). | No<br><input checked="" type="checkbox"/> | Uncertain<br><input type="checkbox"/> | Yes<br><input type="checkbox"/> |

### NEPA Action: Categorical Exclusion

The Proposed Action is covered by the exclusion category and no extraordinary circumstances exist. The Action is excluded from further documentation in an EA or EIS.

## **Attachment A: Cultural Resources Determination**





Bruce, Brandee <bbruce@usbr.gov>

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## SR 201 & 216 Three Bridges CR Comments

1 message

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**Perry, Lauren** <lperry@usbr.gov>

Tue, Mar 17, 2015 at 8:30 AM

To: philip.vallejo@dot.ca.gov

Cc: Brandee Bruce <bbruce@usbr.gov>, Benjamin Lawrence <blawrence@usbr.gov>

Philip,

This email concerns the designation of lead Federal agency for the State Route 201 and 216 Three Bridges Bridge Replacement Project, proposed in Tulare County, California. One location proposed in the project is on and adjacent to the Reclamation-owned Friant-Kern Canal. Reclamation's undertaking is to authorize work within the Friant-Kern Canal and its adjacent right-of-way for one of the bridges. Although we have been in conversations with Caltrans regarding compliance with Section 106 of the National Historic Preservation Act (NHPA) and were working under the presumption that Caltrans would take the lead for NHPA compliance, we did not formally do this designation.

Reclamation hereby designates the California Department of Transportation (Caltrans) as the lead Federal agency to act on behalf of Reclamation for the purposes of compliance with Section 106 of the National Historic Preservation Act (NHPA) for this undertaking.

We request that your next submission to SHPO includes Reclamation's undertaking and designation of lead agency. If this is not done, then Reclamation will need to consult separately on our undertaking prior to issuing the authorization. Provide us with copies of all SHPO correspondence. Our tracking number is 14-SCAO-161 for reference on correspondence.

Attached are comments on the Draft Finding of Effect document we received from you. Thank-you for the opportunity to comment.

Laureen

—  
Laureen M. Perry, MA, RPA  
Regional Archaeologist  
Bureau of Reclamation  
Mid-Pacific Regional Office  
2800 Cottage Way  
Sacramento, CA 95825  
916-978-5028  
916-335-3816 (cell)



**Reclamation Review of First Draft 14-SCAO-161.docx**

24K

**OFFICE OF HISTORIC PRESERVATION  
DEPARTMENT OF PARKS AND RECREATION**

1725 23<sup>rd</sup> Street, Suite 100  
SACRAMENTO, CA 95816-7100  
(916) 445-7000 Fax: (916) 445-7053  
calshpo@parks.ca.gov  
www.ohp.parks.ca.gov



March 19, 2015

Reply in Reference To: FHWA\_2013\_1220\_001

Jeanne Day Binning, PhD  
Chief, Central California Cultural Resources  
Caltrans, District 06  
855 M Street, Suite 200  
Fresno, CA 93721

RE: Finding of Effect for the State Route 201 and 216 Bridge Rail Replacement Project; 06-TUL-201, PM 12.4 and 21.2 and 06-TUL-216, PM 18.68; Tulare County, California.

Dear Dr. Binning:

Thank you for your letter of March 18, 2015, continuing consultation regarding the above noted undertaking in accordance with the *First Amended Programmatic Agreement Among the Federal Highway Administration, the Advisory Council on Historic Preservation, the California State Historic Preservation Officer, and the California Department of Transportation Regarding Compliance with Section 106 of the National Historic Preservation Act, as it Pertains to the Administration of the Federal-Aid Highway Program in California* (PA). You are consulting with me regarding effects that the above project will have on the Friant-Kern canal, determined eligible to the National Register of Historic Places (NRHP) in 1997, and for prehistoric site CA-TUL-27, which has been assumed eligible to the NRHP for purposes of this project only.

The California Department of Transportation (Caltrans) is proposing to replace existing bridge rails and widen three bridges by twelve feet at three locations on State Route (SR) 201 and SR 216 in Tulare County, California. You have submitted the *Finding of No Adverse Effect for the State Route 201 and 216 Bridge Widening and Rail Replacement Project in Tulare County, California* (Caltrans, March 2015); in support of Caltrans' *Finding of No Adverse Effect* for the project. The *Finding of No Adverse Effect* (Caltrans, March 2015) also contained an *Archaeological Monitoring Plan* and *ESA Action Plan* as Appendixes.

Caltrans has determined the proposed construction activities associated with the project at Location 2 (Friant-Kern Canal) are consistent with Secretary of Interior Standards for bridge rehabilitation. Bridge No. 46-0065 will retain sufficient historical integrity to properly convey its historical context as a utilitarian bridge associated with the Friant-Kern Canal.

Archaeological site CA-TUL-27 will be protected from adverse impacts by designation as an Environmentally Sensitive Area (ESA) and the installation of ESA fencing and traffic cones to prevent access to the site during construction. The intact buried stratum within the APE will be protected and marked as a vertical ESA and no excavation will be allowed at that location. Archaeological monitoring by qualified personnel and members of the Native American

community will be conducted for all ground disturbing activities in and adjacent to site boundaries.

Caltrans has determined a *Finding of No Adverse Effect* to historical properties by the proposed project. I concur with Caltrans assessment of effects however, pursuant to Stipulation X.B.1. I find a *Finding of No Adverse Effect with Standard Conditions / ESA Action Plan* for the proposed project to be more appropriate. I have the following additional comments:

1. I suggest a procedure should be put in place to disseminate the information regarding cultural resources addressed in the preconstruction meeting to field personnel who did not attend the preconstruction meeting; such as new hires, substitutes, etc.
2. The Archaeological Monitoring Plan states that monitoring is not needed on the bank of the Kaweah River opposite from CA-TUL-27 based on information obtained from a geo-archaeological analysis at that location. However, the geo-archaeological study did find an historic trash pit during trenching. It is unreasonable to assume that there is no circumstance that would result in a cultural deposit being preserved. There is also information suggesting that an ethno-historic village was located on the east side of the river at that location. For these reasons, I think that the monitoring area should be extended to the east side of the river where the most earth moving will occur during construction.

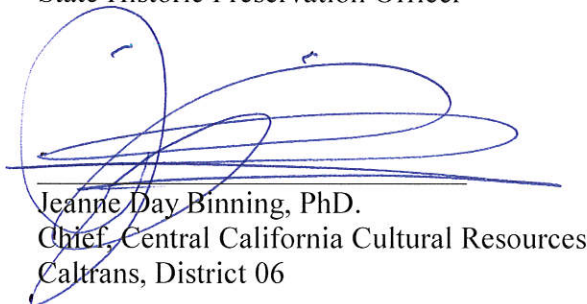
If Caltrans agrees with the changes cited above. Please sign at the bottom of this letter and return a copy to me.

Be advised that under certain circumstances, such as unanticipated discovery or a change in the project description, Caltrans may have additional future responsibilities for this undertaking under 36 CFR Part 800. Thank you for seeking my comments and considering historic properties as part of your project planning. If you have any questions or concerns regarding archaeological resources, please contact Associate State Archaeologist, Kim Tanksley at (916) 445-7035 or by email at [kim.tanksley@parks.ca.gov](mailto:kim.tanksley@parks.ca.gov). For questions regarding the built environment, please contact State Historian, Natalie Lindquist at (916)445-7014 or by email at [natalie.lindquist@parks.ca.gov](mailto:natalie.lindquist@parks.ca.gov).

Sincerely,



Carol Roland-Nawi, PhD  
State Historic Preservation Officer



Jeanne Day Binning, PhD.  
Chief, Central California Cultural Resources  
Caltrans, District 06

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION  
CALIFORNIA  
REVISOR BY  
DATE  
DESIGNED BY  
CHECKED BY  
FUNCTIONAL SUPERVISOR

DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET TOTAL SHEETS
06	Tul	201	21.10/21.31	

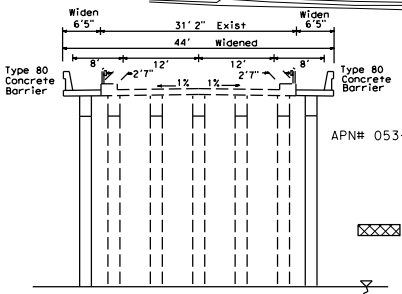
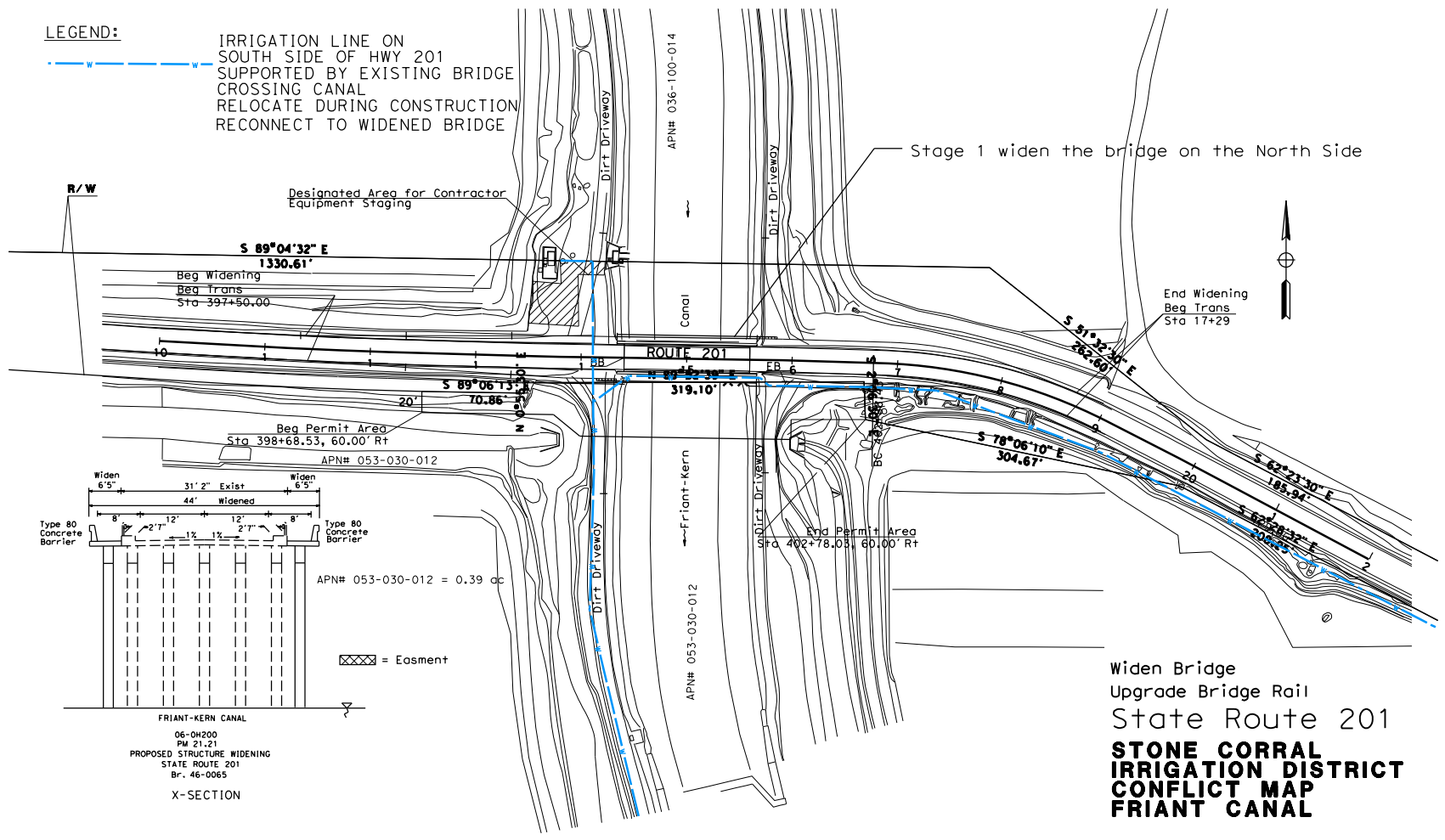
REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF SCANNED COPIES OF THIS PLAN SHEET.

LEGEND:

IRRIGATION LINE ON SOUTH SIDE OF HWY 201 SUPPORTED BY EXISTING BRIDGE CROSSING CANAL RELOCATE DURING CONSTRUCTION RECONNECT TO WIDENED BRIDGE



Widen Bridge  
Upgrade Bridge Rail  
State Route 201  
**STONE CORRAL  
IRRIGATION DISTRICT  
CONFLICT MAP  
FRIANT CANAL**

STAGE 1

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

REVISOR  
DATE

CALCULATED  
DESIGNED BY  
CHECKED BY

FUNCTIONAL SUPERVISOR

DATE



DIST	COUNTY	ROUTE	POST MILES TOTAL PROJECT	SHEET TOTAL SHEETS
06	Tul	201	21.10/21.31	

REGISTERED CIVIL ENGINEER DATE

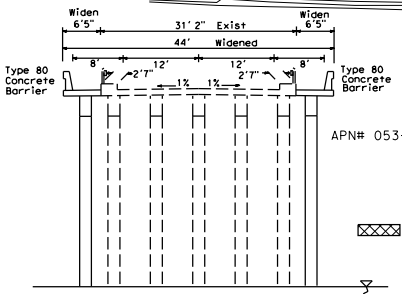
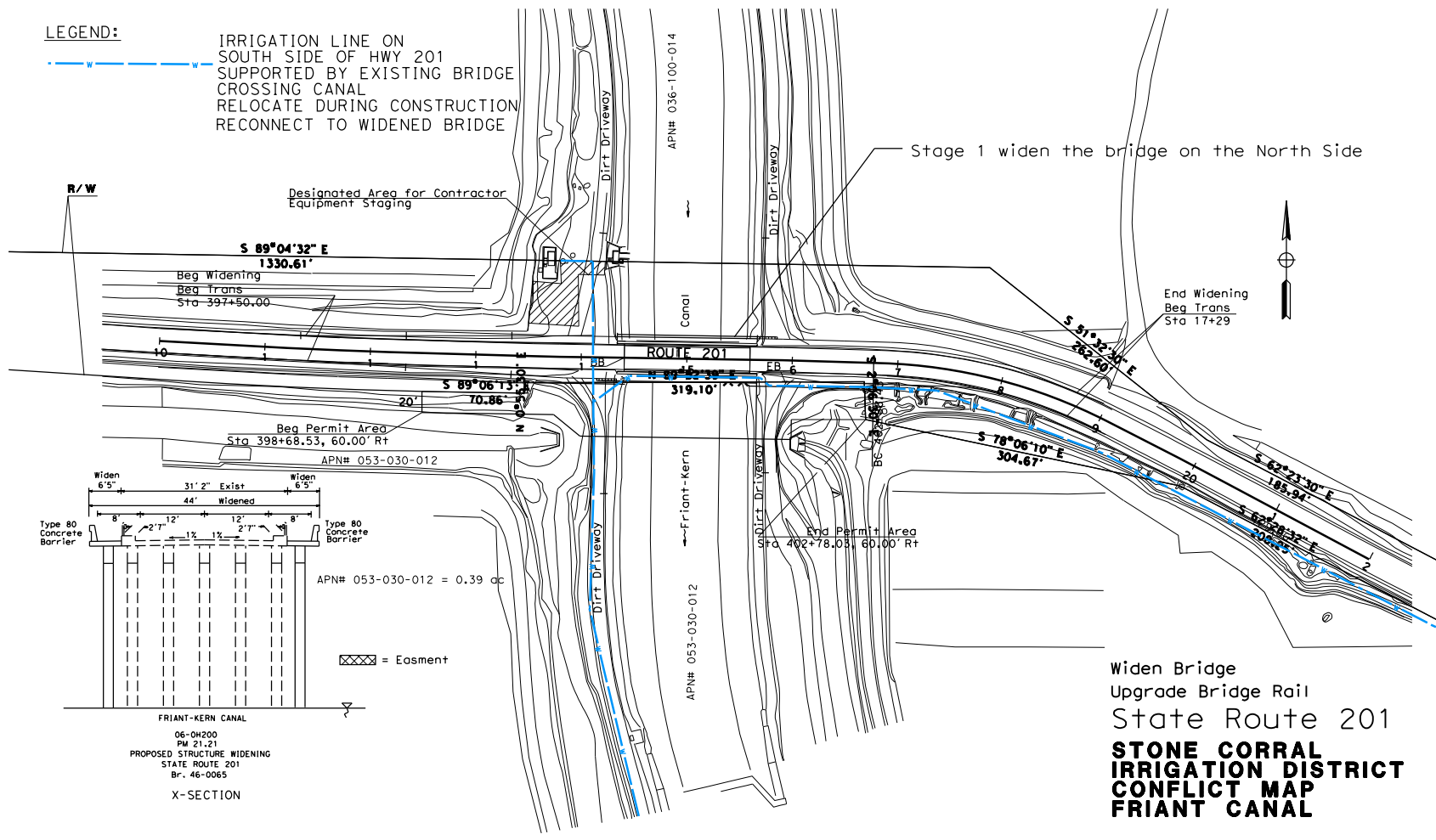
PLANS APPROVAL DATE

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LEGEND:

IRRIGATION LINE ON SOUTH SIDE OF HWY 201

SUPPORTED BY EXISTING BRIDGE  
CROSSING CANAL  
RELOCATE DURING CONSTRUCTION  
RECONNECT TO WIDENED BRIDGE

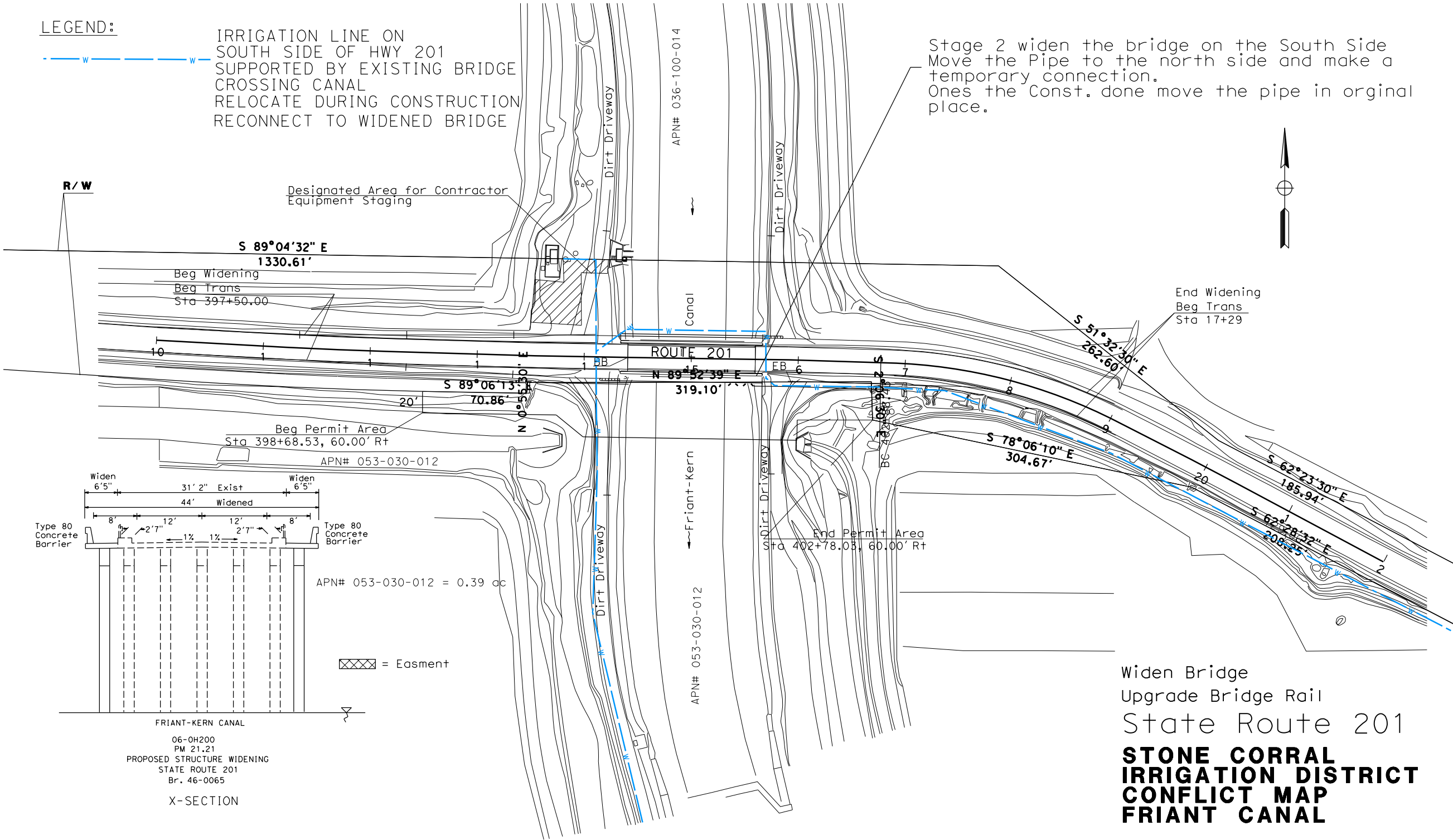


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STAGE 1



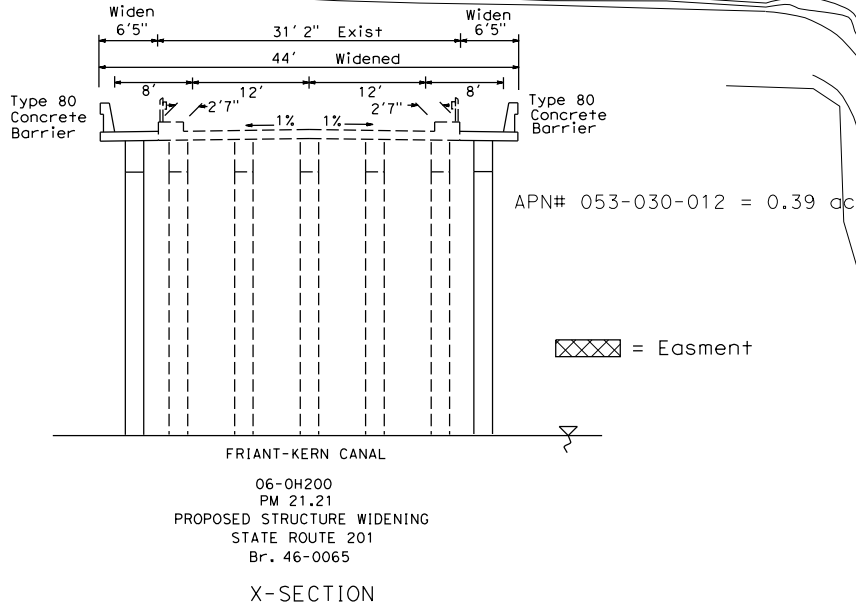
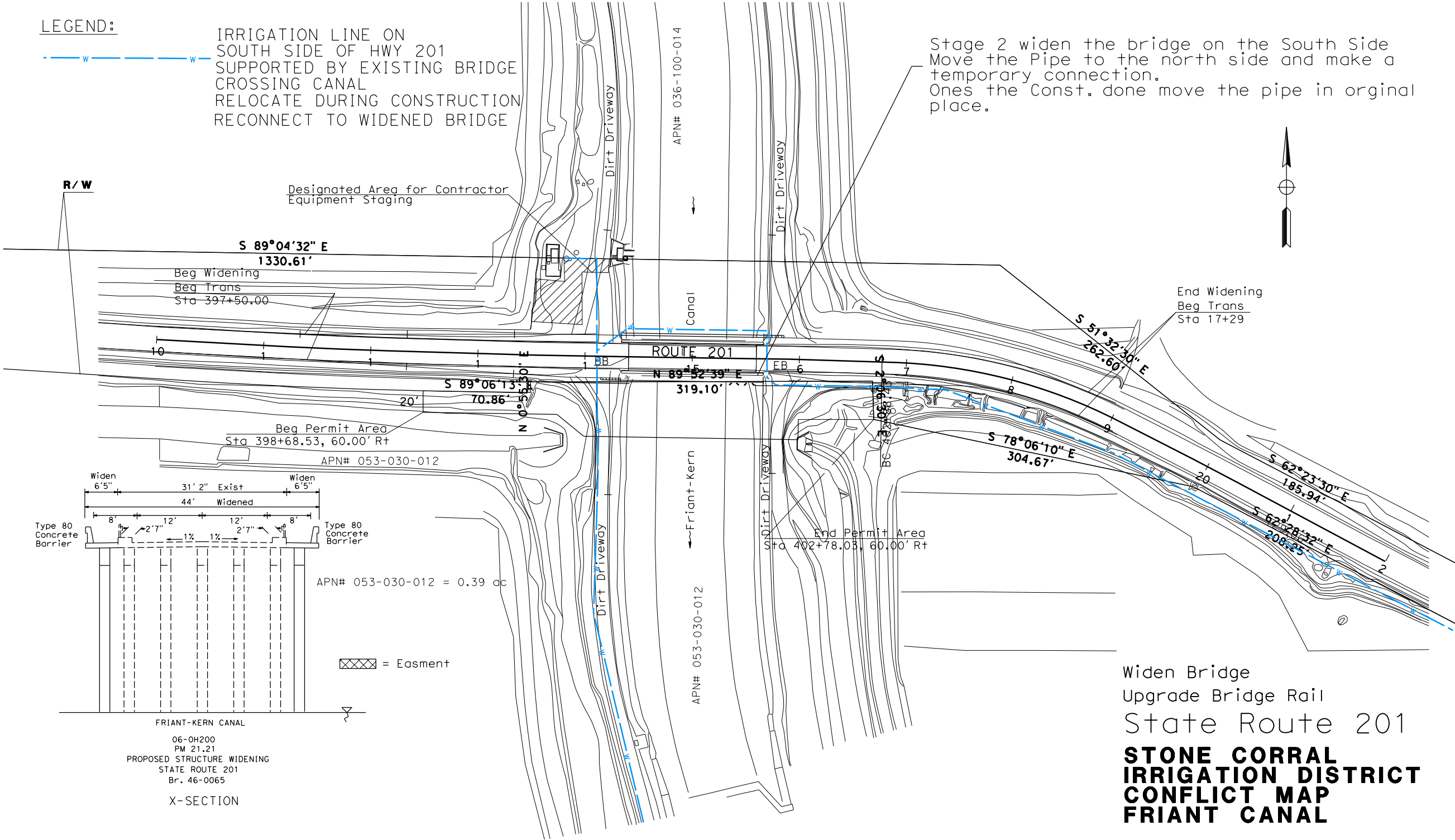


LEGEND:

— W — W —

IRRIGATION LINE ON SOUTH SIDE OF HWY 201 SUPPORTED BY EXISTING BRIDGE CROSSING CANAL RELOCATE DURING CONSTRUCTION RECONNECT TO WIDENED BRIDGE

Stage 2 widen the bridge on the South Side Move the Pipe to the north side and make a temporary connection. Ones the Const. done move the pipe in orginal place.



Widen Bridge  
Upgrade Bridge Rail  
State Route 201  
**STONE CORRAL  
IRRIGATION DISTRICT  
CONFLICT MAP  
FRIANT CANAL**

## **Attachment C**

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### Reclamation's Indian Trust Assets Determination





Lawrence, Benjamin &lt;blawrence@usbr.gov&gt;

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## SCCAO CEC 14-052 Widening of SR 201 over the Friant-Kern Canal

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**RIVERA, PATRICIA** <privera@usbr.gov>

Wed, Sep 24, 2014 at 10:13 AM

To: "Lawrence, Benjamin" &lt;blawrence@usbr.gov&gt;, Kristi Seabrook &lt;kseabrook@usbr.gov&gt;

Ben,

I reviewed the proposed action to approve Caltrans' proposal to widen the bridge on SR 201 over the Friant-Kern Canal in Tulare County. Bridge railings would also be upgraded, and it may be necessary to modify piers within the canal prism for additional structural support. Reclamation would modify the existing long-term crossing authorization and provide a short-term construction easement. Work is proposed to begin in fall of 2016.

The proposed action does not have a potential to impact Indian Trust Assets.

Patricia Rivera  
Native American Affairs Program Manager  
US Bureau of Reclamation  
Mid-Pacific Region  
2800 Sacramento, California 95825  
(916) 978-5194

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Kristi please log in. No further action needed. Thanks